

Norman Comprehensive Transportation Plan

THE CTP PLAN

CPTC Committee
February 24, 2014

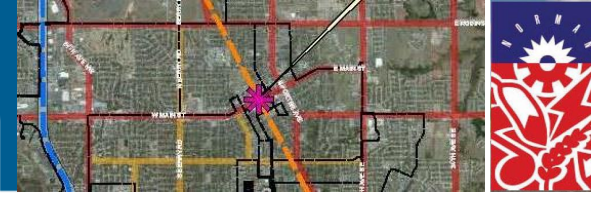
Transportation Planning for Moving Forward



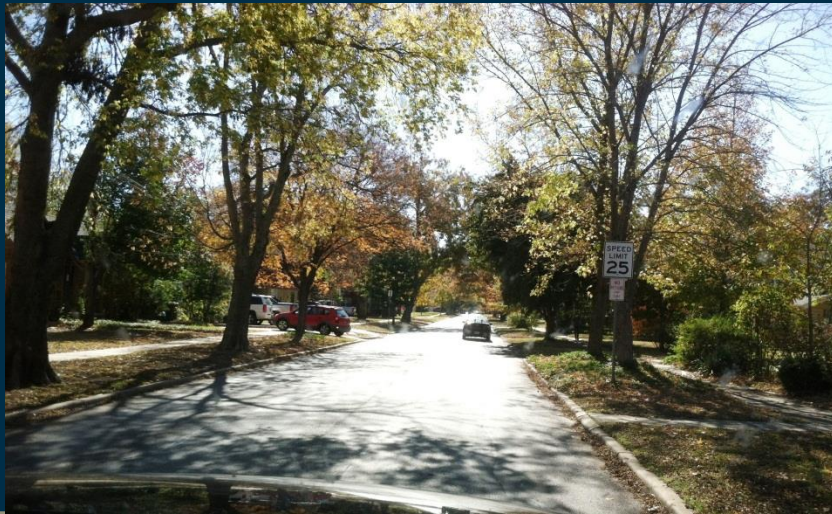
- **Framework for growth**
- **Land Use/transportation interface**
- **Multi-modal considerations**
- **System Alignments/ROW Preservation/Design Standards**
- **Coordination with other agency/city plans**
- **Infrastructure and utilities coordination**
- **Capital Improvements Programming**
- **Funding of Improvements**
- **Economic benefit**
- **Statement of Community Policy**



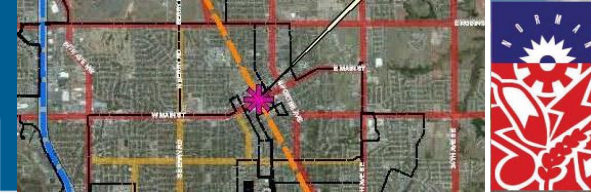
Benefits of Transportation Planning



- **Informed Public**
- **Increased Mobility, Options and Safety**
- **Facilitate Growth and Development**
- **Community Connectivity**
- **Sensitivity to Land Planning**



Project Overview



- CTP Input and Research
 - Existing System
 - System Needs
- Sub-Committee Work
- Plan Concepts and Refinement
- Modal Plans
- Draft CTP Document
 - Basis for the Plan
 - Transportation System Needs
 - Transportation System Master Plan
 - System Implementation Plan

CTP Planning Process

1. Guiding Principles, Goals & Objectives

2. Assessment of Existing Systems

3. Assessment of Needs

4. Travel Forecast Modeling

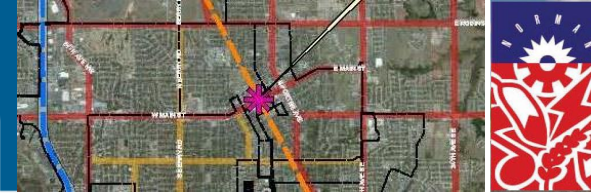
5. Transportation Plan and Prioritization

6. Transportation Policies and Programs

7. Implementation

8. Documentation

Guiding Principles, Goals, Objectives



Special Place to Live

- **Vibrant** Norman Community in 2035
- Transportation and Infrastructure **focus on both people and places**
- Enhanced transportation **choices** and **accessibility**
- Create a **unique place** with **lasting value**
- **Blends** seamlessly with the character of **Norman's neighborhoods, employment centers and activity centers**

Mobility

- Seamless system of **transportation options** and solutions
- Norman Moving Forward's emphasis on **system management and operations, context sensitive** and **complete streets designs**
- Range of **accessible and convenient, multi-modal transportation choices** that provide connections between neighborhoods and destinations

Maintain and Preserve Existing Infrastructure

- Priority on **maintenance, rehabilitation, safety** and **reconstruction**
- **Neighborhood viability** through **maintaining streets, sidewalks, utilities, storm water systems** and other infrastructure facilities
- Investments **balance transportation needs** of the community and local neighborhoods

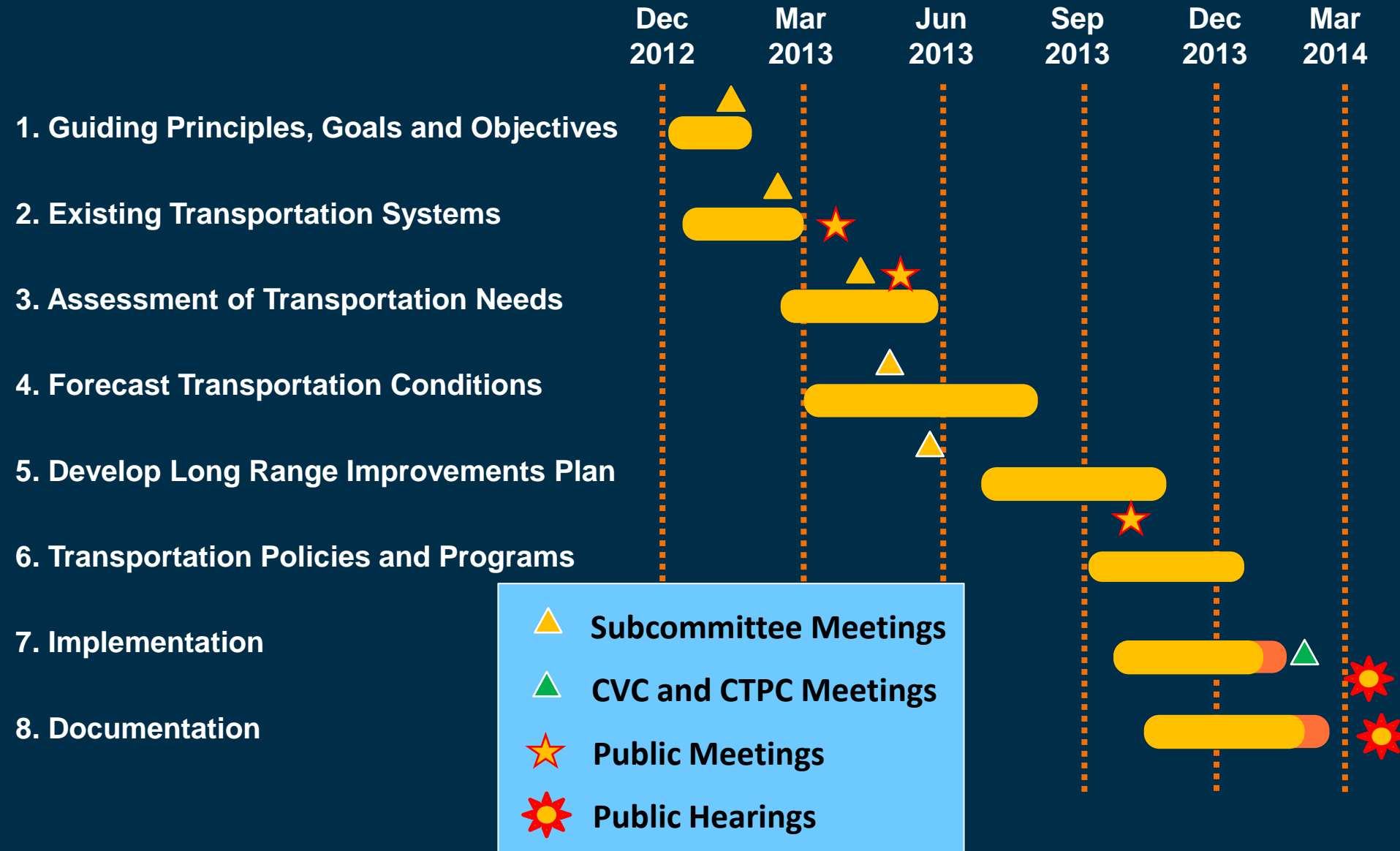
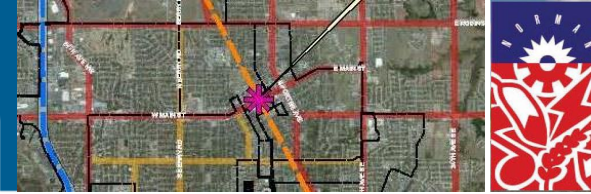
Fiscal Stewardship





- Provide a **detailed roadmap of actions** for transportation and infrastructure improvements
- Investments **maximize the benefits for multiple user groups** in a way that is **fiscally and environmentally responsible**
- **Input** from the **community-at-large** and ongoing dialogue with **stakeholders**

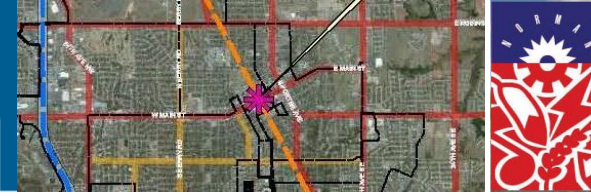
Enhance Economic Vitality

- Promotes economic growth while **using resources in an efficient and effective manner**
- Supports a **diverse, vibrant local economy** with a **strong tax base**
- **Reduces the fiscal burden on residents** to provide city services

Public Input to the CTP



-  Subcommittee Meetings
-  CVC and CTPC Meetings
-  Public Meetings
-  Public Hearings



Norman Comprehensive Transportation Plan

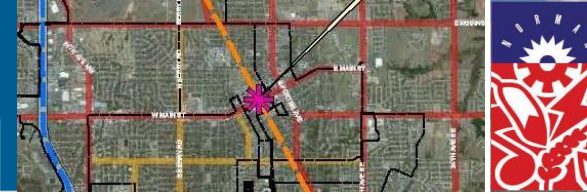
A Multi-Modal Plan for 2035.

Modal Elements














- *Thoroughfare Plan and Typical Sections*
- *Bike & Pedestrian Plan, Sidewalk Completion Plan*
- *Transit Service Plan*
- *Airport, Freight and Emergency Response*

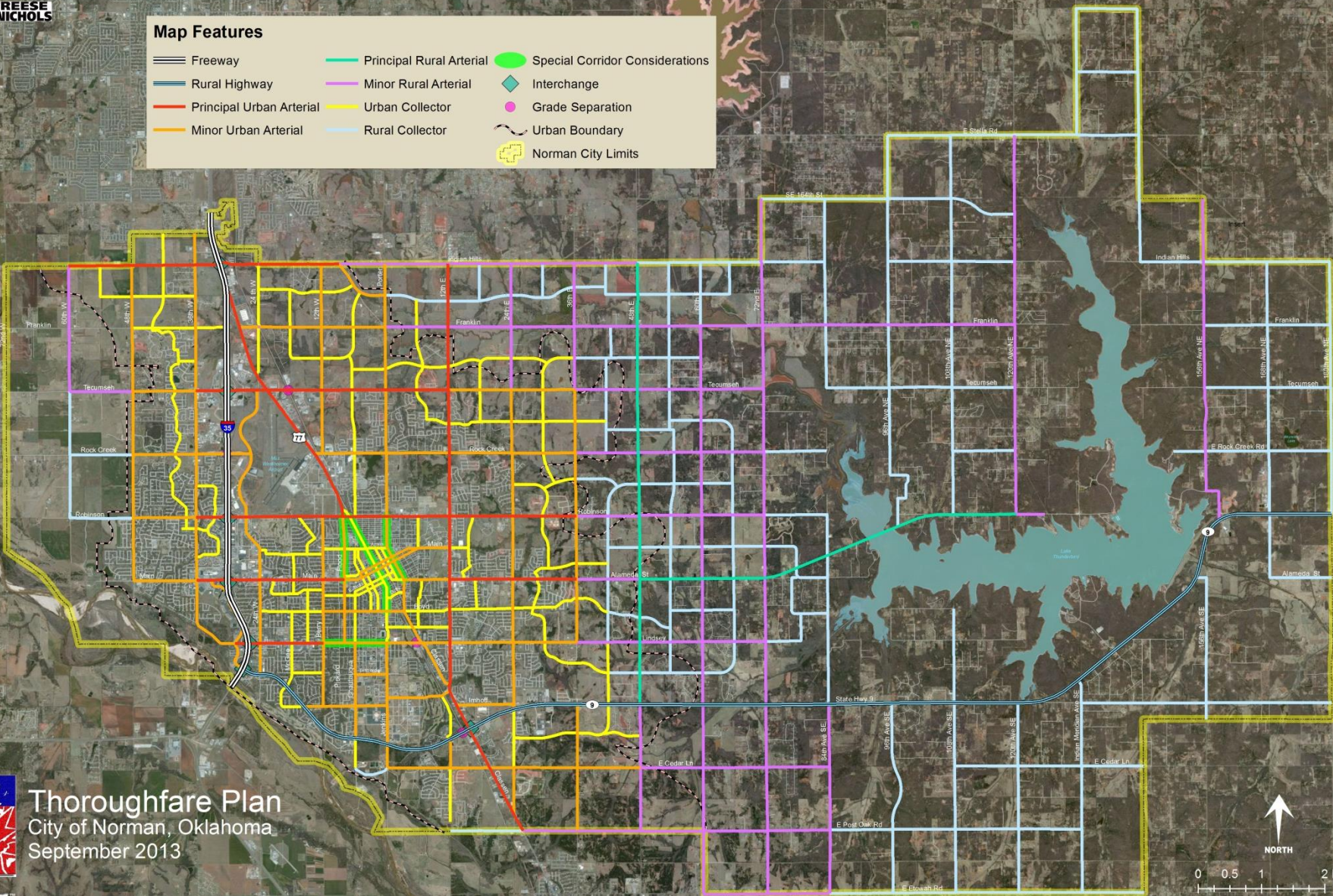


Thoroughfare Plan



Map Features

-  Freeway
-  Rural Highway
-  Principal Urban Arterial
-  Minor Urban Arterial
-  Principal Rural Arterial
-  Minor Rural Arterial
-  Urban Collector
-  Rural Collector
-  Special Corridor Considerations
-  Interchange
-  Grade Separation
-  Urban Boundary
-  Norman City Limits



Thoroughfare Plan
City of Norman, Oklahoma
September 2013

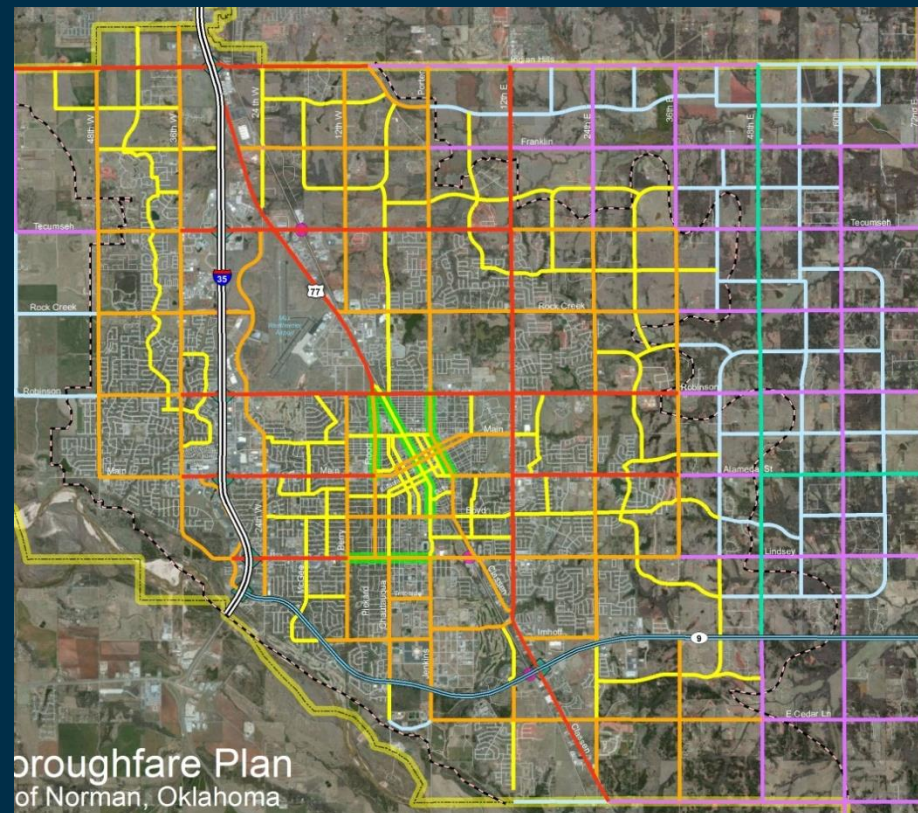
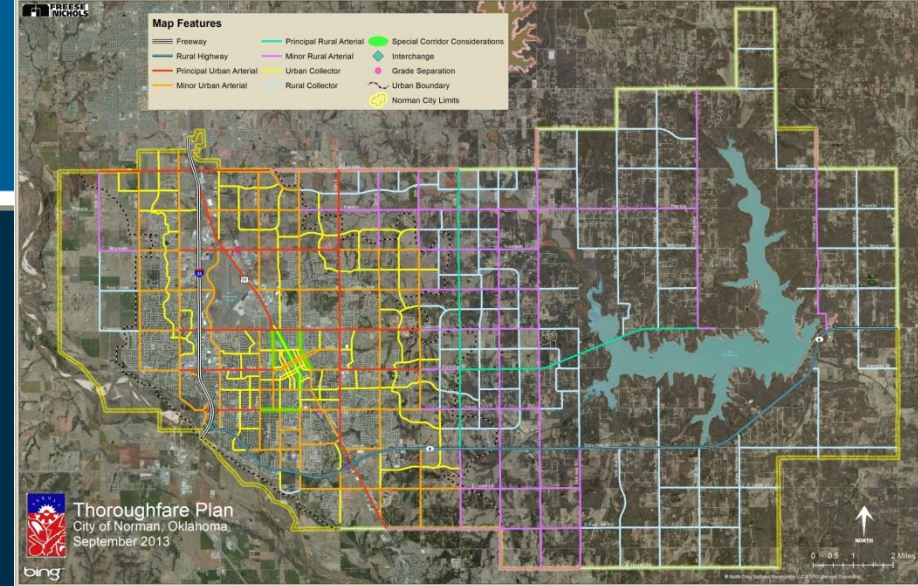


0 0.5 1 2 Miles

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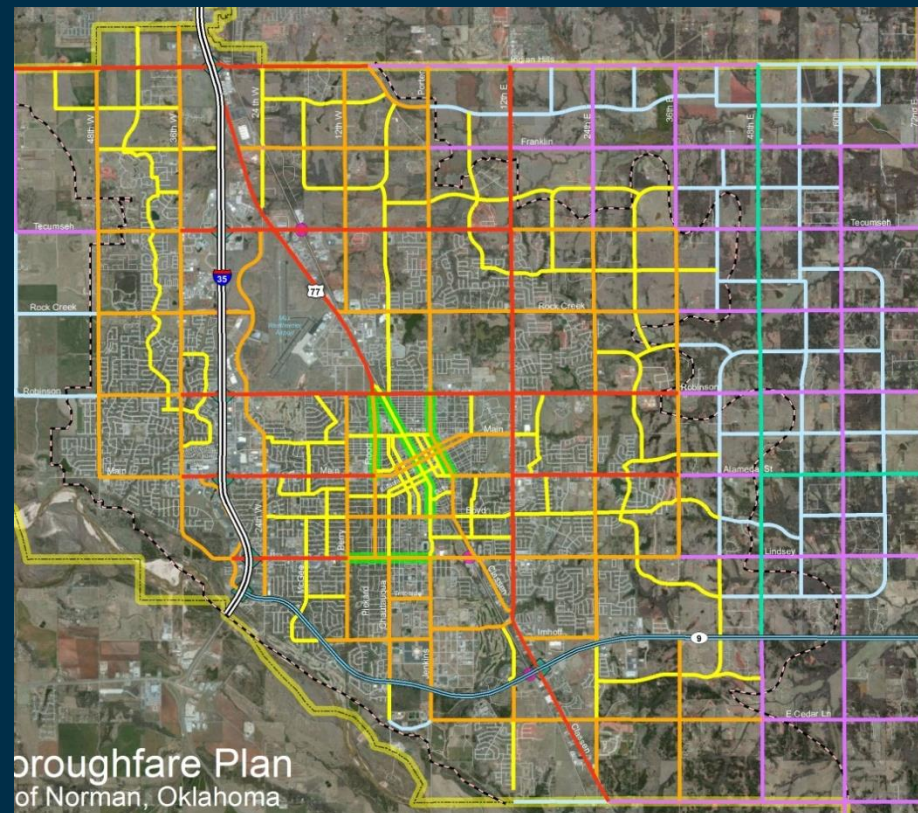
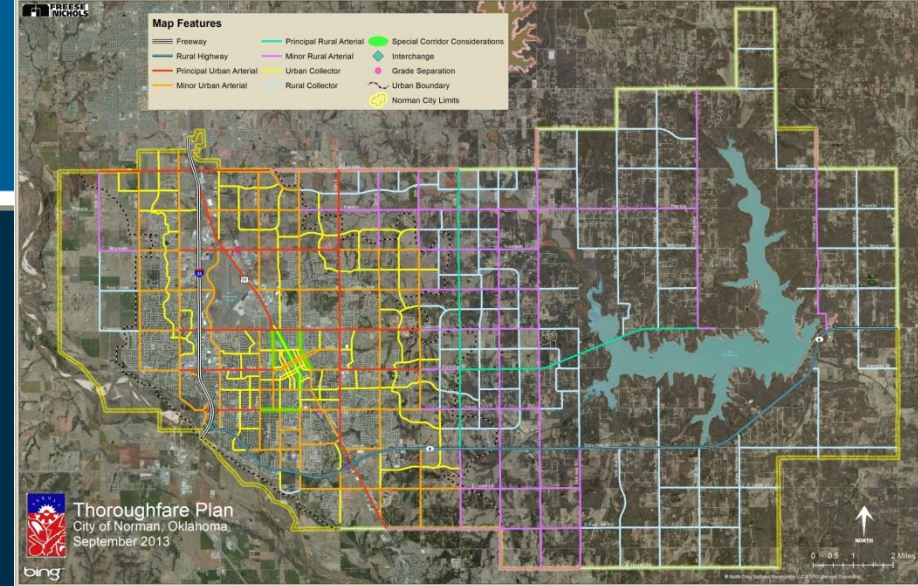
Highlights of the Thoroughfare Plan

- Added definition to functional classification and network
- Design options for roadway sections
- Formalization of internal loop for regional connection
- Identification of Collector network supporting section grid
- Creation of Special Corridors
- Railroad grade separations at Tecumseh and Lindsey



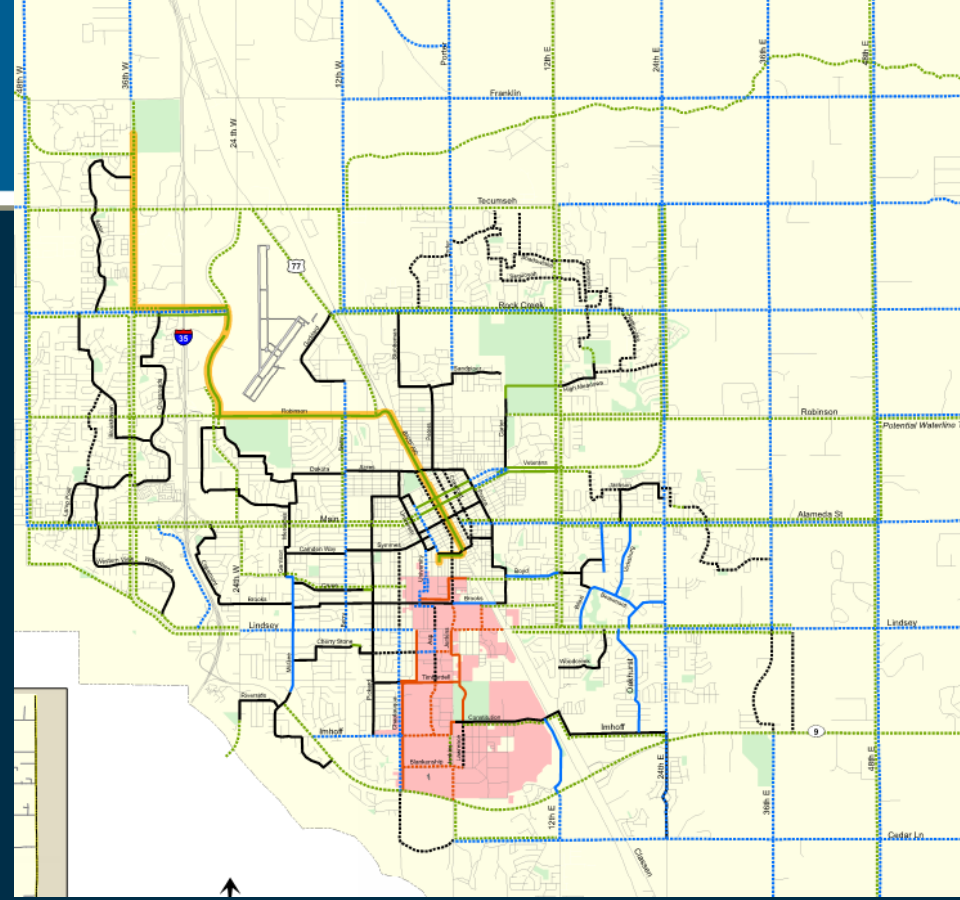
Highlights of the Thoroughfare Plan

- Extension of James Garner
- Increased significance of Jenkins and Chautauqua from SH 9
- Re-thinking Main and Gray Streets thru downtown
- Main/Gray (Porter to Roundabout)
- Special Corridor Considerations
 - Lindsey Street
 - Porter Avenue
 - Flood Avenue
 - James Garner
- Rural Principal Arterials
 - 48th Avenue East
 - Alameda Street



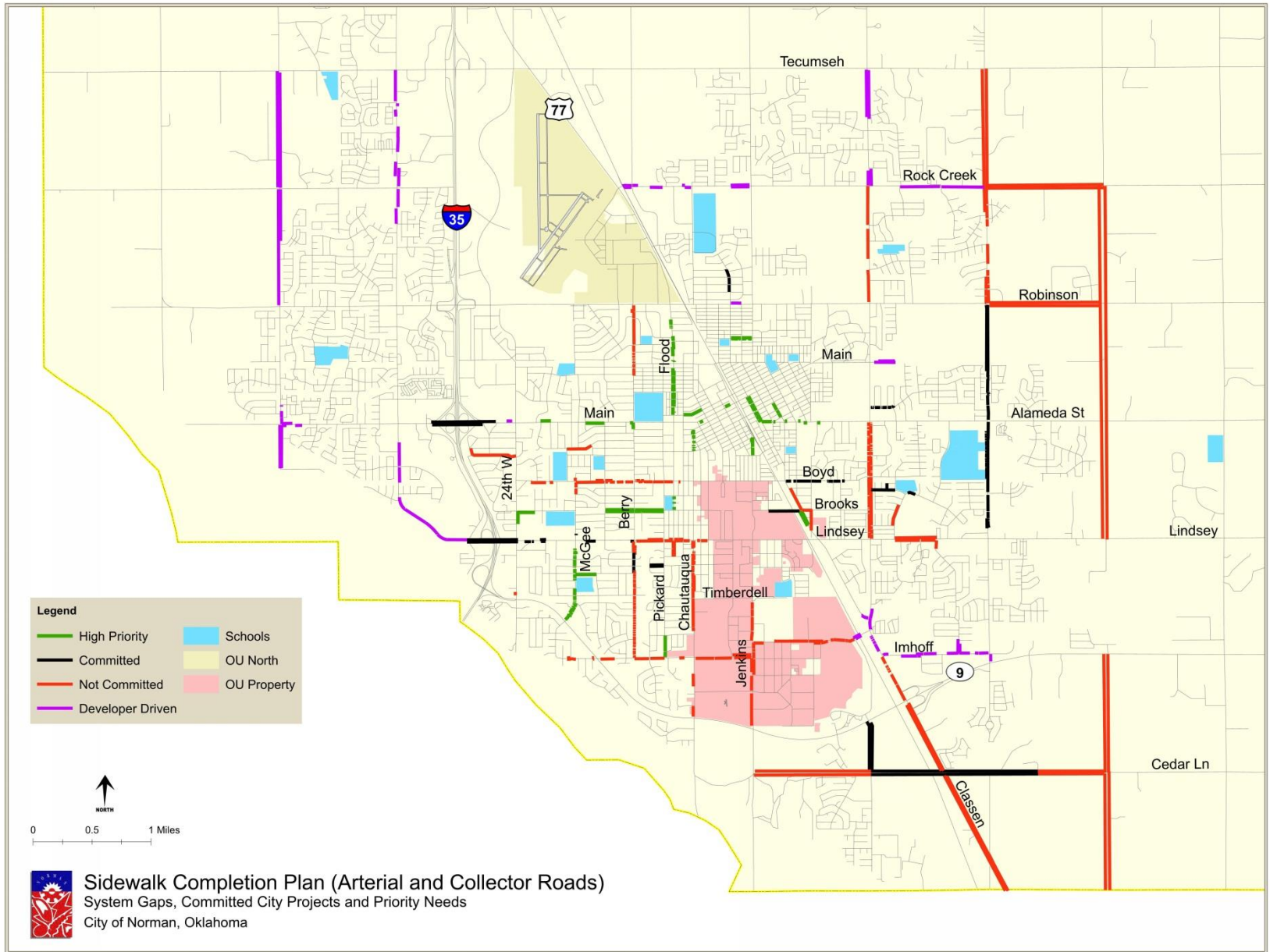
Highlights of the Bike & Pedestrian Plan

- Side-paths: 8-10' along roadway
 - 12th Ave. E
 - 48th Ave. E (thru rural Norman)
 - Lindsey (12th Ave. E to Classen)
 - Robinson /Main corridor
- Multi-use paths: 10-12' off road
 - Extension of Legacy Trail around Max Westheimer Airport
 - SH 9 Path
 - Robinson Waterline Path
- On-Street Facility Standards
 - Bike lanes on urban minor arterials
 - Shoulder bike lanes on all rural principal/minor arterials



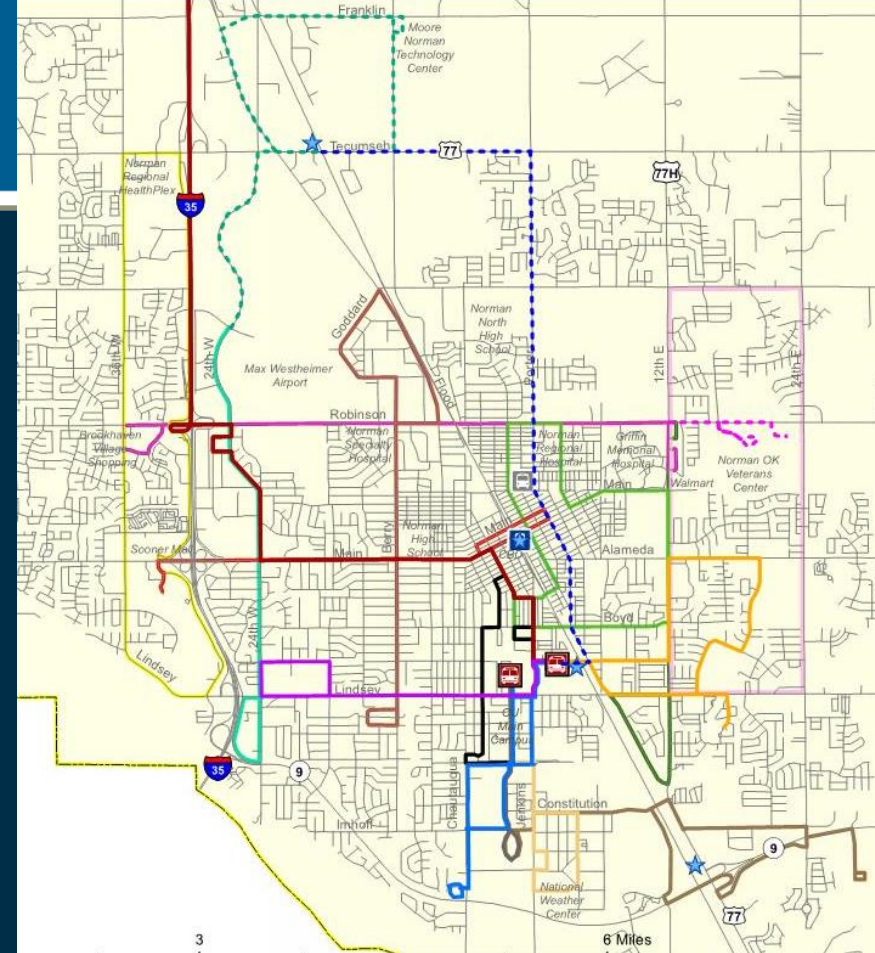
- Notable Bike Lanes along:
 - Berry Road (Lindsey to Robinson)
 - Lindsey (Elm to 24th Ave. W)
 - Rock Creek Road, E and W of Airport
 - University/Webster

Sidewalk Completion Plan



Highlights of the Transit Plan

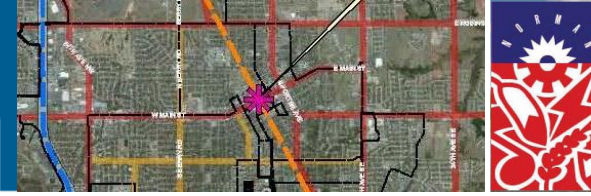
- 2008 CARTS Plan; enhanced for:
 - Porter-Classen corridor
 - Moore Norman Technology Center
 - University North Park
 - SH9/Cedar Lane area
- Increased service frequency and hours of operation
- Better accommodation of patrons with wheelchairs
- ADA compliance at stops
- Regional Commuter Rail Stations
 - Tecumseh, Downtown, SH9/Imhoff
 - Platform for OU special events
- IH35 – Reversible HOV lane concept



Airport, Freight and Emergency Response Needs



- Protect zoning around airport and industrial districts to serve freight, rail and air transportation needs
- Additional lane capacity for special events
- Additional grade separated crossing with RR
- Corridor enhancements
- Land use coordination



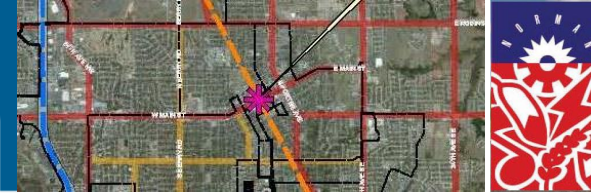
- **Implementation Strategy**
 - Immediate, Ongoing
 - Short Range (0 to 5 years)
 - Medium Range (5 to 10 years)
 - Long Range (10 to 20 years)
- **Actions to Achieve CTP Goals**
 - Institute design standards and procedures
 - Focus on Goals & Objectives, Performance Measures
- **Prioritized Projects and Programs**
 - Special Corridors
 - Strategic funding opportunities
- **Funding Sources**
 - Local and Public – Private Partnerships
 - Regional & State
 - Federal

What We Heard from The Citizen Visioning Committee



- Review/Comment on CTP Recommendations
 - Thoroughfare Plan:
 - Specific issues on RR grade separations, associated cost and prioritization
 - Discussion of key corridors, potential timing and configuration relative to other recommended projects
 - Considerations for project prioritization.
 - Transit Plan:
 - Incorporation of 2008 CARTS plan, need to have more incremental projects (“Pilot projects”) to advance current transit to recommended system.
 - Approval to commuter rail station locations
 - Bike Plan: Specific feedback on system additions.
- Comments to Implementation and Funding Strategies
 - General approval of short/medium/long-range prioritization.
 - Any new funding strategies need to be evaluated relative to current policies and benchmarking with other municipalities in the region.

Next Steps Going Forward



- Public Hearing Process (Tentative)
 - Planning Commission
(March 13, 2014)
 - City Council
(March 18, 2014)

*Comprehensive Transportation Plan
Upcoming Meetings
February 2014 - March 2014*

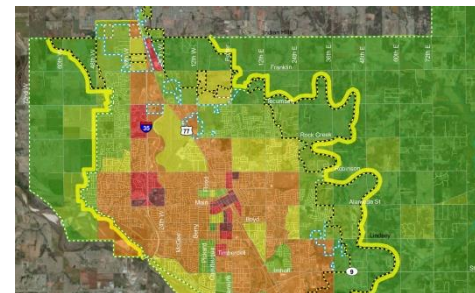
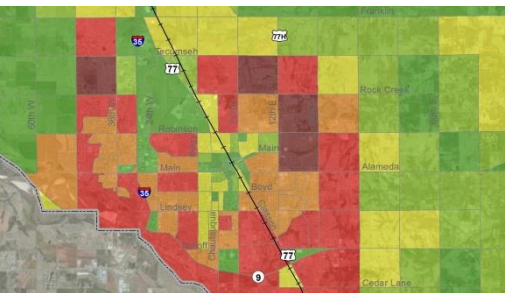
	Staff Team Meeting	CTP Visioning Committee Meeting	Presentation to CPTC Committee	Staff Team Meeting - Debrief	Planning Commission	Council Study Session	Council Adoption
Monday, February 24, 2014	9:00 -12:00 noon	3:00 p.m.	5:30 p.m.				
Tuesday, February 25, 2014				9:00 a.m.			
Thursday, March 13, 2014	2:00 -4:00 p.m.				6:30 p.m.		
Tuesday, March 18, 2014	2:00 - 4:00 p.m.					5:00 p.m.	
Date to be determined	—————→						

Your Thoughts and Comments.....



- Key CTP Recommendations
- CTP Modal Plans
- CTP Implementation Strategies





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CTPC Committee
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